



STATE OF NORTH CAROLINA  
DEPARTMENT OF TRANSPORTATION

ROY COOPER  
GOVERNOR

J. ERIC BOYETTE  
SECRETARY

February 17, 2021

CONTRACT: DB00505  
WBS ELEMENT: 2021CPT.02.34.20541  
COUNTY: LENOIR  
ROUTE: VARIOUS SECONDARY ROUTES  
DESCRIPTION: MILLING, STRENGTHENING, RESURFACING, AND  
SHOULDER RECONSTRUCTION OF DETOUR ROUTES OF  
B-5619 IN LENOIR COUNTY

**ADDENDUM 1**

TO: PROSPECTIVE BIDDERS

Map 4 (SR 1389 – Hardy Bridge Road) resurfacing has been removed from this contract.  
Please note the following revisions to the proposal.

- Revised Notes to Contractors on Page 24 of the proposal removing references to Map 4.
- Clarification on the 4” Mill Patching Payment stating that this item includes the asphalt binder on Page 31 of the proposal.
- Revision to the project plans removing Map 4.
- Please see the attached page A1-A13
- A revised electronic file has been uploaded to bid express named DB00505.001.

**Please make sure to sign the addendum page in the proposal to acknowledge this addendum.**

**Due to COVID-19, I am working remotely. If you need to contact me, please email at [mmoore@ncdot.gov](mailto:mmoore@ncdot.gov).**

Sincerely,

DocuSigned by:  
*Mary Voelker Moore*  
714C11DCCEBC4C6...

Mary Voelker Moore, PE  
Division Contract Engineer

cc: Mr. Jeremy Stroud, PE  
Ms. Mary Beth Houston, PE  
Mr. Cadmus Capehart, PE  
Ms. Heather Lane, PE  
Mr. Aaron Bullard, PE  
Mr. Jeff Cabaniss, PE

**PROJECT SPECIAL PROVISIONS**

**ROADWAY**

**NOTE TO CONTRACTOR**

Map # 1 (SR 1300) includes the resurfacing of Bridge # 44 with 2.5" of ACIC, Type I19.0C and 1.5" of ACSC, Type S9.5C as shown on Typical Section #1.

Map # 2 (SR 1311) includes the resurfacing of Bridge # 49 with 1.5" of ACSC, Type S9.5C. Contractor shall taper the Intermediate Course to existing asphalt before and after bridge as determined by the Engineer.

**SHOULDER RECONSTRUCTION PER SHOULDER MILE:**

(11-16-10) (Rev. 8-21-12)

560

SPI R07AR (Rev)

**Description**

This work consists of reconstructing each shoulder (including median shoulders as applicable) in accordance with Standard Drawing No. 560.01 and 560.02 of the *2018 Roadway Standard Drawings* except that the rate of slope and width will be as shown on typical section, or to the existing shoulder point, whichever is nearer, as long as the desired typical is achieved, and when completed, seeding and mulching. This work shall be performed immediately after the resurfacing operations are complete as directed by the Engineer.

This project will require the removal of excess material from the existing pavement by mechanical means prior to paving routes. Excess material generated by clipping excessive shoulder material from the existing pavement will be required to be removed by the Contractor. The Contractor should take care in removing excessive material from the existing pavement to minimize the amount of disturbance to adjacent established vegetation. There will be no direct payment for the removal of any excess material generated from the existing pavement as this work shall be considered incidental to the asphalt pavement line items that correspond with that map.

Quantities for Shoulder Reconstruction are included to address drop off issues identified by the Engineer after paving is completed. Shoulder Reconstruction shall not be performed unless directed by the Engineer.

**Materials**

The NCDOT will furnish all earth material necessary for the construction of the shoulders at the Lenoir County Jones Pitt located across from 2992 NC 903 in LaGrange.

- B. 95 / 90 denotes that 95% of the coarse aggregate has one fractured face and 90% has 2 or more fractured faces.

**4” MILL PATCHING:**

**Description**

The Contractor's attention is directed to the fact that there are areas of existing pavement on this project that will require repair prior to resurfacing. 4” mill patch the areas that, in the opinion of the Engineer, need repairing. The areas for 4” mill patching will be delineated by the Engineer prior to the Contractor performing repairs.

**Materials**

The 4” mill patching consists of Asphalt Concrete Base Course, Type B25.0C

**Construction Methods**

Work shall be performed by a milling machine for 4” mill patching operations at the depth, width and locations as shown in the plan sheets in accordance with Section 607-3 and 610-9 of the *2018 Standard Specifications*.

Schedule operations so that all areas where 4” mill patching has been performed will be repaired on the same day and all lanes of traffic restored.

**Measurement and Payment**

*4” Mill Patching* will be measured and paid as the actual number of tons of Asphalt Concrete Base Course, Type B25.0C in place that has been used to make completed and accepted repairs. The asphalt plant mixed material will be measured by being weighed in trucks on certified platform scales or other certified weighing devices. The unit price will be full compensation for all work covered by this provision, including but not limited to removal and disposal of all types of pavement; removal and disposal of all types of sub grade material; furnishing and applying tack coat; furnishing, placing, and compacting of asphalt plant mix. This item includes the asphalt binder.

Payment will be made under:

<b>Pay Item</b>	<b>Pay Unit</b>
Generic Paving Item (4” Mill Patching With Asphalt Conc Base Course, Type B25.0C)	Ton

**PORTLAND CEMENT CONCRETE PRODUCTION AND DELIVERY:**

(9-15-20)

1000, 1014, 1024

SP10 R01

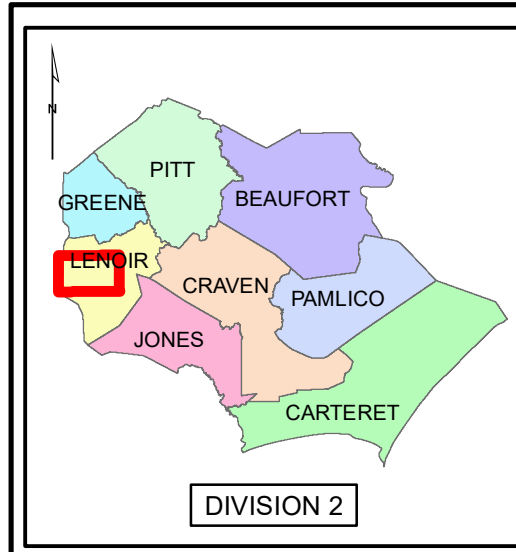
Revise the *2018 Standard Specifications* as follows:

**Page 10-6, Table 1000-1, REQUIREMENTS FOR CONCRETE**, replace with the following:

<b>TABLE 1000-1 REQUIREMENTS FOR CONCRETE</b>
---

County : Lenoir

Line #	Item Number	Sec #	Description	Quantity	Unit Cost	Amount
<b>ROADWAY ITEMS</b>						
0001	0000100000-N	800	MOBILIZATION	Lump Sum	L.S.	
0002	0262000000-N	SP	GENERIC GRADING ITEM HAULING NCDOT SUPPLIED SHOULDE R MATERIAL	421 EA		
0003	1220000000-E	545	INCIDENTAL STONE BASE	350 TON		
0004	1245000000-E	SP	SHOULDER RECONSTRUCTION	14.02 SMI		
0005	1330000000-E	607	INCIDENTAL MILLING	2,150 SY		
0006	1491000000-E	610	ASPHALT CONC BASE COURSE, TYPE B25.0C	6,550 TON		
0007	1503000000-E	610	ASPHALT CONC INTERMEDIATE COURSE, TYPE 119.0C	14,300 TON		
0008	1523000000-E	610	ASPHALT CONC SURFACE COURSE, TYPE S9.5C	8,000 TON		
0009	1575000000-E	620	ASPHALT BINDER FOR PLANT MIX	1,460 TON		
0010	1880000000-E	SP	GENERIC PAVING ITEM 4" DEPTH MILL PATCHING EXISTIN G PAVEMENT B 25.0 C	575 TON		
0011	4413000000-E	SP	WORK ZONE ADVANCE/GENERAL WARNING SIGNING	825 SF		
0012	4457000000-N	SP	TEMPORARY TRAFFIC CONTROL	Lump Sum	L.S.	
0013	6000000000-E	1605	TEMPORARY SILT FENCE	900 LF		
0014	6071010000-E	SP	WATTLE	400 LF		
0015	6084000000-E	1660	SEEDING & MULCHING	8.77 ACR		
0016	6117000000-N	SP	RESPONSE FOR EROSION CONTROL	1 EA		



**LENOIR COUNTY**  
**DB00505**

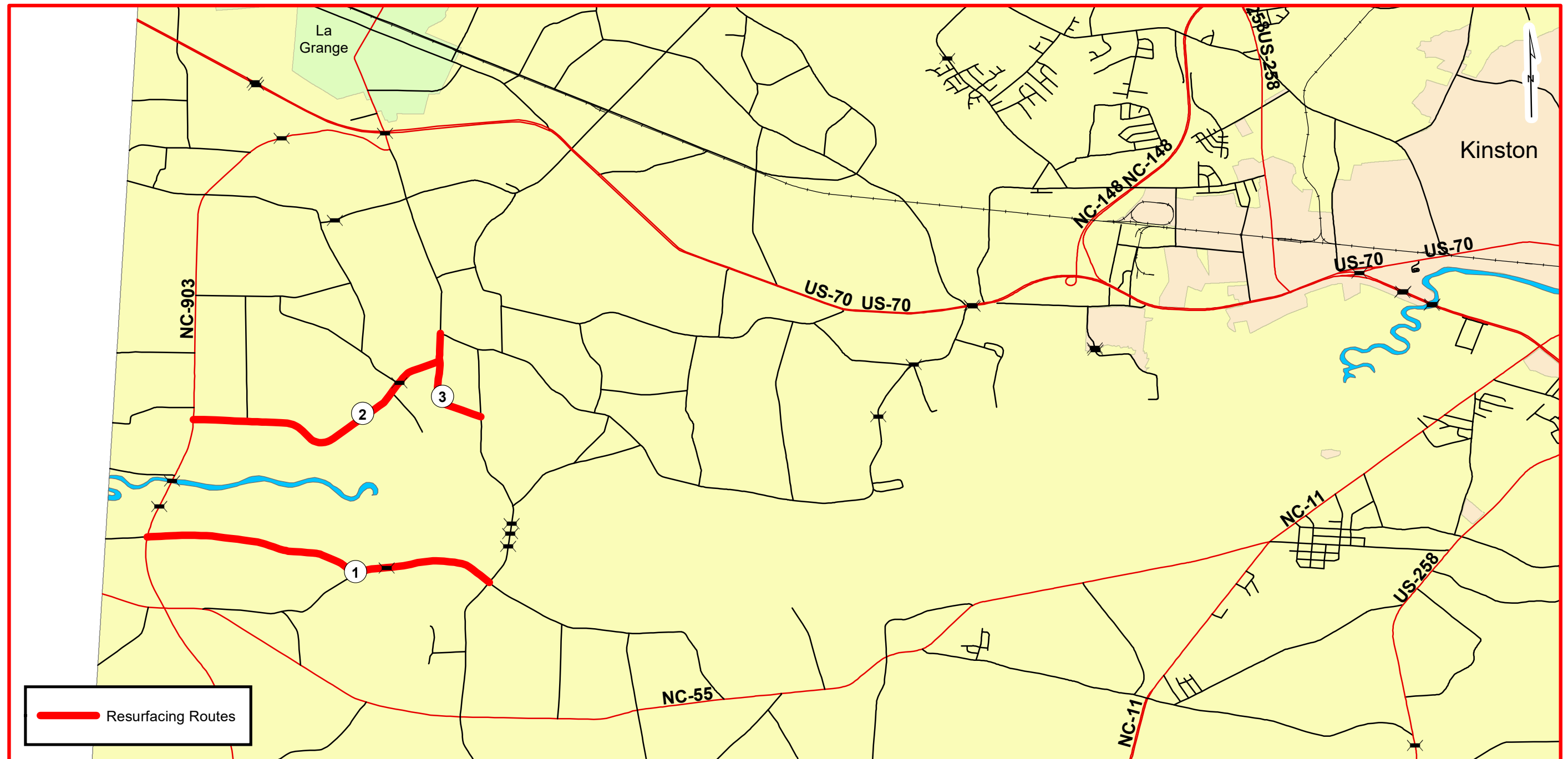
**WBS# 2021CPT.02.34.20541**

PROJECT REFERENCE NO.	SHEET NO.
DB00505	1

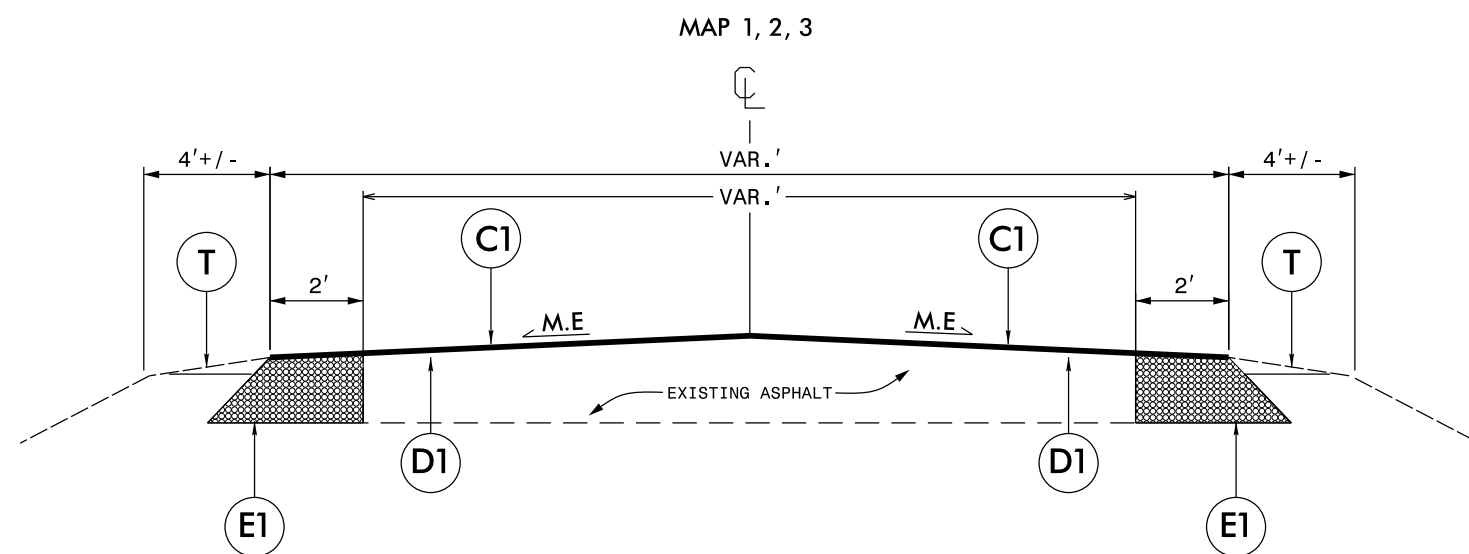


**NCDOT**  
DIVISION 2

**TYPE OF WORK: WIDENING, STRENGTHENING, RESURFACING, SHOULDER RECONSTRUCTION**



### TYPICAL SECTION NO. 1



**NOTE:**

1. PLACE ASYMMETRICAL WIDENING, AS DIRECTED BY THE ENGINEER. MAKE FLUSH WITH THE EXISTING ASPHALT.
2. INCLUDES INCIDENTAL MILLING AT THE ENDS OF MAIN LINE AND Y-LINE SECTIONS, AS DIRECTED BY THE ENGINEER.
3. PLACE ASPHALT INTERMEDIATE COURSE AT FULL WIDTH, INCLUDING NEW WIDENING
4. PLACE ASPHALT SURFACE COURSE AT FULL WIDTH, INCLUDING NEW WIDENING.
5. PERFORM SHOULDER RECONSTRUCTION AFTER PAVING IS COMPLETED.

### PAVEMENT SCHEDULE

C1	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
D1	PROP. APPROX. 2 1/2" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD.
E1	PROP. APPROX. 6" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C AT AN AVERAGE RATE OF 684 LBS. PER SQ. YD.
T	SHOULDER RECONSTRUCTION
V1	INCIDENTAL MILLING.

**DRAWINGS NOT TO SCALE**

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.

PROJECT NO.	SHEET NO.	TOTAL NO.
DB00505	3	

**SUMMARY OF QUANTITIES**

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	0262000000-N	1220000000-E	1245000000-E	1330000000-E	1491000000-E	1503000000-E	1523000000-E	1575000000-E	1880000000-E	6000000000-E	6071010000-E	6084000000-E	6117000000-N	
												HAULING NCDOT SUPPLIED SHOULDER MATERIAL	INCIDENTAL STONE BASE	SHOULDER RECONSTRUCTION	INCIDENTAL MILLING	BASE COURSE, B25.0C	INTERMEDIATE COURSE, I19.0C	SURFACE COURSE, S9.5C	ASPHALT BINDER FOR PLANT MIX	4" DEPTH MILL PATCHING EXISTING PAVEMENT - B 25.0 C	TEMPORARY SILT FENCE	WATTLE	SEED & MULCHING	RESPONSE FOR EROSION CONTROL	
											MI	FT	EA	TONS	SMI	SY	TONS	TONS	TONS	TONS	TON	LF	LF	AC	EA
2021CPT.02.34.20541	Lenoir	1	SR 1300 DAVIS HARDY RD	FROM NC 903 TO SR 1389	1	2	2WU	NO	NO	3.30	18	198	165	6.60	1,250	3,400	6,800	3,800	707		500	200	4.13	1	
<b>TOTAL FOR MAP NO. 1</b>											<b>3.30</b>		<b>198</b>	<b>165</b>	<b>6.60</b>	<b>1,250</b>	<b>3,400</b>	<b>6,800</b>	<b>3,800</b>	<b>707</b>		<b>500</b>	<b>200</b>	<b>4.13</b>	<b>1</b>
2021CPT.02.34.20541	Lenoir	2	SR 1311 BEAR CREEK RD	FROM NC 903 TO SR 1309	1	2	2WU	NO	NO	2.58	18	155	129	5.16	400	2,850	5,400	3,000	567	575	300	160	3.23		
<b>TOTAL FOR MAP NO. 2</b>											<b>2.58</b>		<b>155</b>	<b>129</b>	<b>5.16</b>	<b>400</b>	<b>2,850</b>	<b>5,400</b>	<b>3,000</b>	<b>567</b>	<b>575</b>	<b>300</b>	<b>160</b>	<b>3.23</b>	
2021CPT.02.34.20541	Lenoir	3	SR 1309 JENNY LIND RD	FROM SR 1389 TO SR 1324	1	2	2WU	NO	NO	1.13	17	68	56	2.26	500	300	2,100	1,200	186		100	40	1.41		
<b>TOTAL FOR MAP NO. 3</b>											<b>1.13</b>		<b>68</b>	<b>56</b>	<b>2.26</b>	<b>500</b>	<b>300</b>	<b>2,100</b>	<b>1,200</b>	<b>186</b>		<b>100</b>	<b>40</b>	<b>1.41</b>	
<b>TOTAL FOR PROJ NO. 2021CPT.02.34.20541</b>											<b>7.01</b>		<b>421</b>	<b>350</b>	<b>14.02</b>	<b>2,150</b>	<b>6,550</b>	<b>14,300</b>	<b>8,000</b>	<b>1,460</b>	<b>575</b>	<b>900</b>	<b>400</b>	<b>8.77</b>	<b>1</b>
<b>GRAND TOTAL</b>											<b>7.01</b>		<b>421</b>	<b>350</b>	<b>14.02</b>	<b>2,150</b>	<b>6,550</b>	<b>14,300</b>	<b>8,000</b>	<b>1,460</b>	<b>575</b>	<b>900</b>	<b>400</b>	<b>8.77</b>	<b>1</b>

MAP	7' FULL DEPTH PATCH - B25.0C - 4"	STATION	STATION	LT	RT
2		37+96	40+20		224'
2		54+36	55+02		66'
2		60+62	62+01	139'	
2		67+98	68+84	86' FULL WIDTH	
2		71+08	74+79		371'
2		81+62	86+35		473'
2		86+35	87+59	124' FULL WIDTH	
2		90+19	91+25	106'	
2		92+05	92+74	69'	
2		95+02	100+02		500'
2		95+16	97+32	216'	
2		127+03	131+04		441'
2		130+13	131+04	91'	

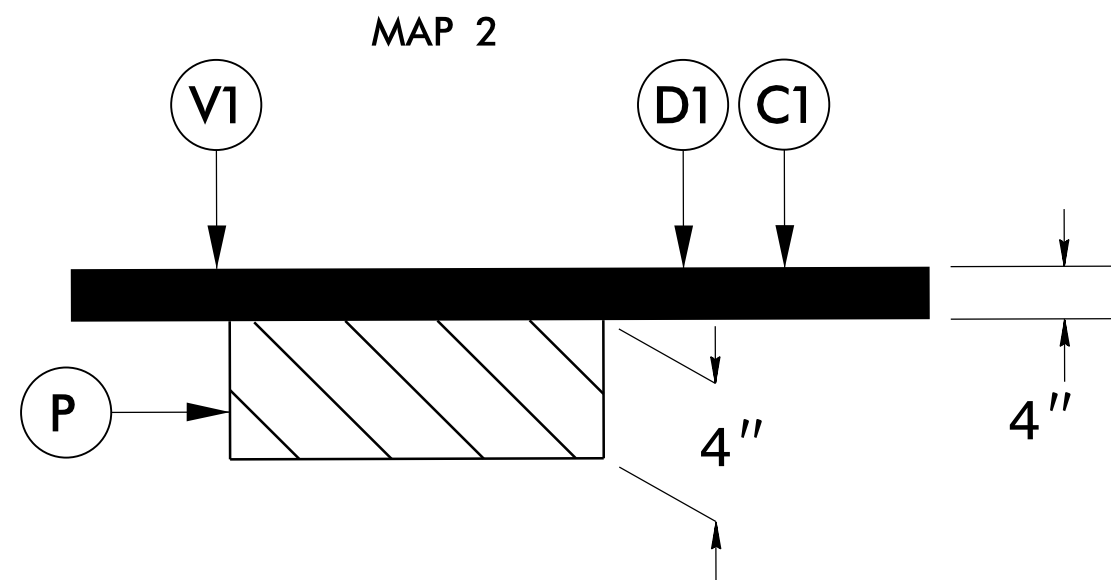


PROJECT NO.	SHEET NO.	TOTAL NO.
DB00505	4	

## TRAFFIC CONTROL

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	4413000000-E		4457000000-N	
								WORK ZONE ADVANCE/GENERAL WARNING SIGNING		TEMPORARY TRAFFIC CONTROL	
								MI	FT	SF	LS
2021CPT.02.34.20541	Lenoir	1	SR 1300 DAVIS HARDY RD	FROM NC 903 TO SR 1389	1	2	2WU	3.30	18	375	0.47
<b>TOTAL FOR MAP NO. 1</b>								<b>3.30</b>		<b>375</b>	<b>0.47</b>
2021CPT.02.34.20541	Lenoir	2	SR 1311 BEAR CREEK RD	FROM NC 903 TO SR 1309	1	2	2WU	2.58	18	300	0.37
<b>TOTAL FOR MAP NO. 2</b>								<b>2.58</b>		<b>300</b>	<b>0.37</b>
2021CPT.02.34.20541	Lenoir	3	SR 1309 JENNY LIND RD	FROM SR 1389 TO SR 1324	1	2	2WU	1.13	17	150	0.16
<b>TOTAL FOR MAP NO. 3</b>								<b>1.13</b>		<b>150</b>	<b>0.16</b>
<b>TOTAL FOR PROJ NO. 2021CPT.02.34.20541</b>								<b>7.01</b>		<b>825</b>	<b>1.00</b>
<b>GRAND TOTAL</b>								<b>7.01</b>		<b>825</b>	<b>1</b>

# 4" DEPTH MILL PATCHING DETAIL

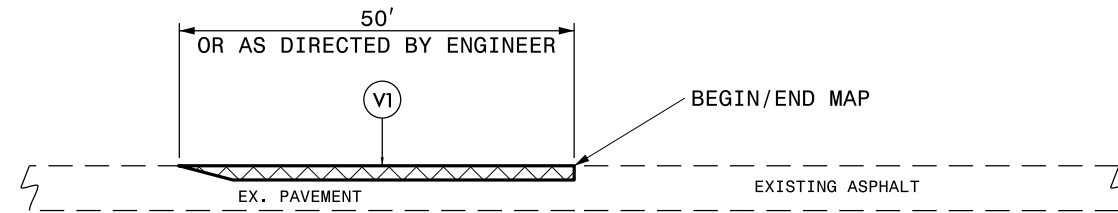


PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
D1	PROP. APPROX. 2 1/2" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD.
V1	INCIDENTAL MILLING
P	4" DEPTH MILL PATCHING W/ B 25.0C
DRAWINGS NOT TO SCALE	

**NOTE:**

1. THE CONTRACTOR SHALL PERFORM ANY UNIFORM OR INCIDENTAL MILLING AT TIE-INS BEFORE PERFORMING THE 4" DEPTH MILL PATCHING.
2. THE CONTRACTOR SHALL PERFORM THE MILL PATCHING REMOVAL AND REPLACEMENT IN THE SAME DAY.
3. 4" DEPTH MILL PATCHING SHALL BE PERFORMED AT LOCATIONS AS SHOWN ON SHEET 3, AND AS DIRECTED BY THE ENGINEER.

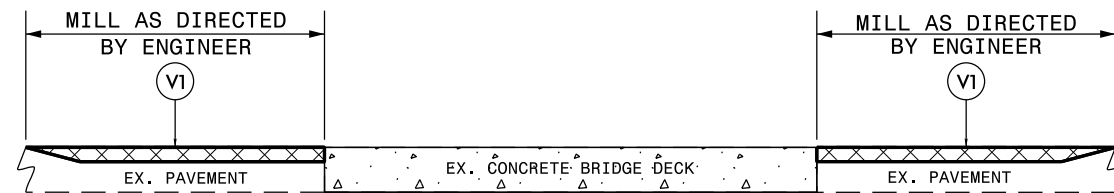
# MILLING TYPICALS



**DETAIL 1**  
BEGIN/END MAP TIE-IN

**NOTE:**

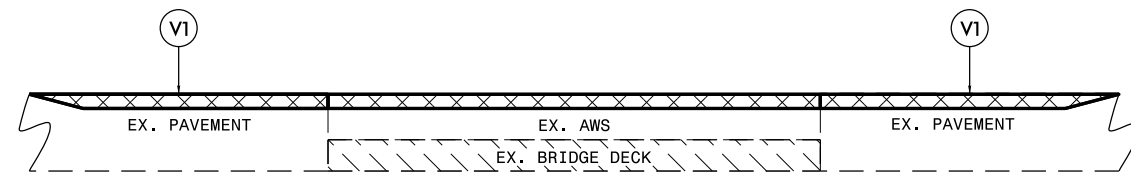
- MILLING SHALL BE PERFORMED AT MAIN LINE TIE-INS AND Y-LINE TIE-INS AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.



**DETAIL 2**  
BRIDGE MILLING

**NOTE:**

- MILLING SHALL BE PERFORMED AT THE BRIDGE APPROACHES AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.

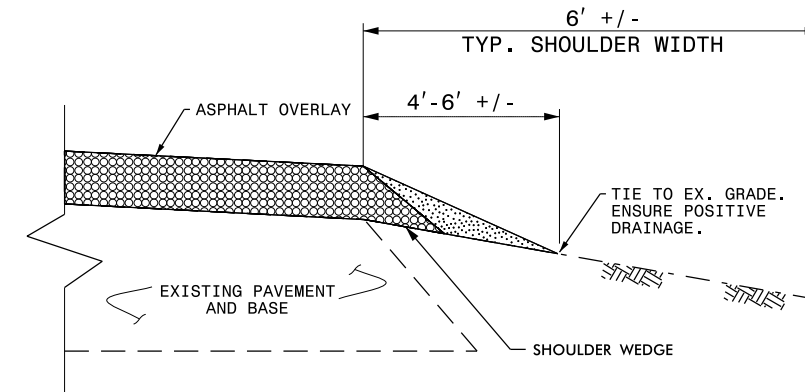


**DETAIL 3**  
BRIDGE MILLING

**NOTE:**

- INCLUDES MILLING FOR THE ENTIRE WIDTH OF THE BRIDGE WEARING SURFACE, AS DIRECTED BY THE ENGINEER.

# SHOULDER RECONSTRUCTION TYPICAL

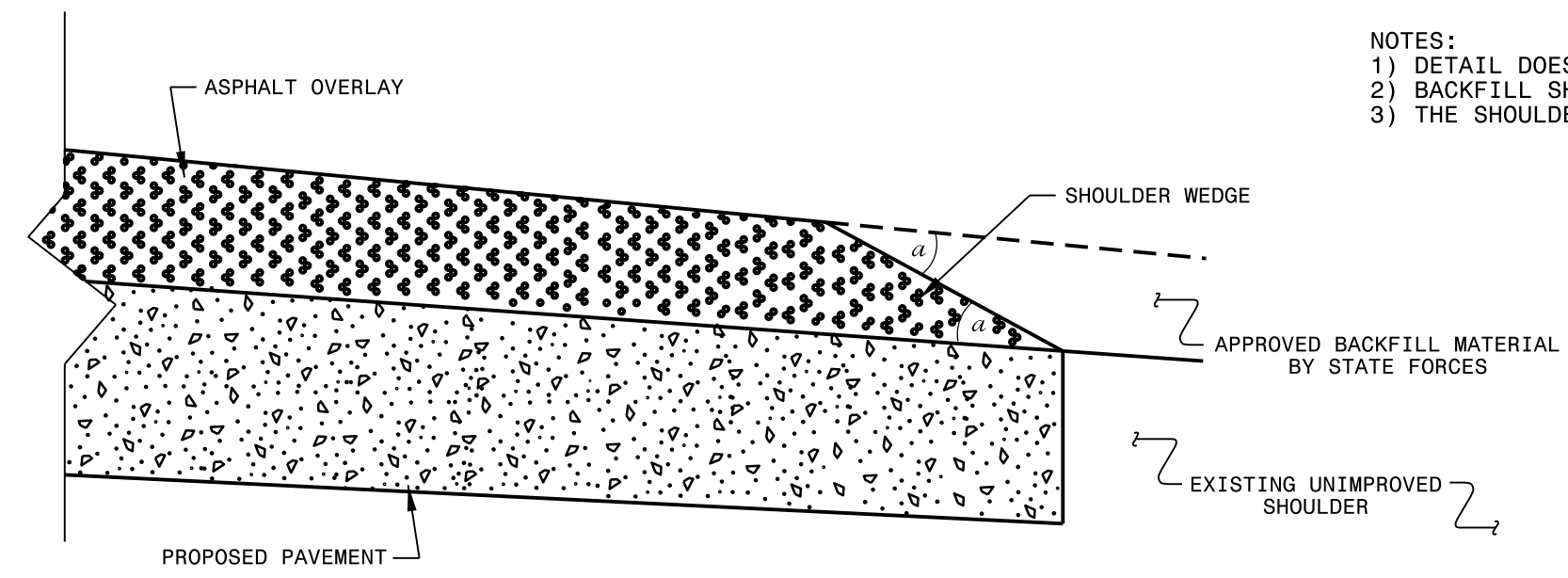


**SHOULDER RECONSTRUCTION DETAIL**

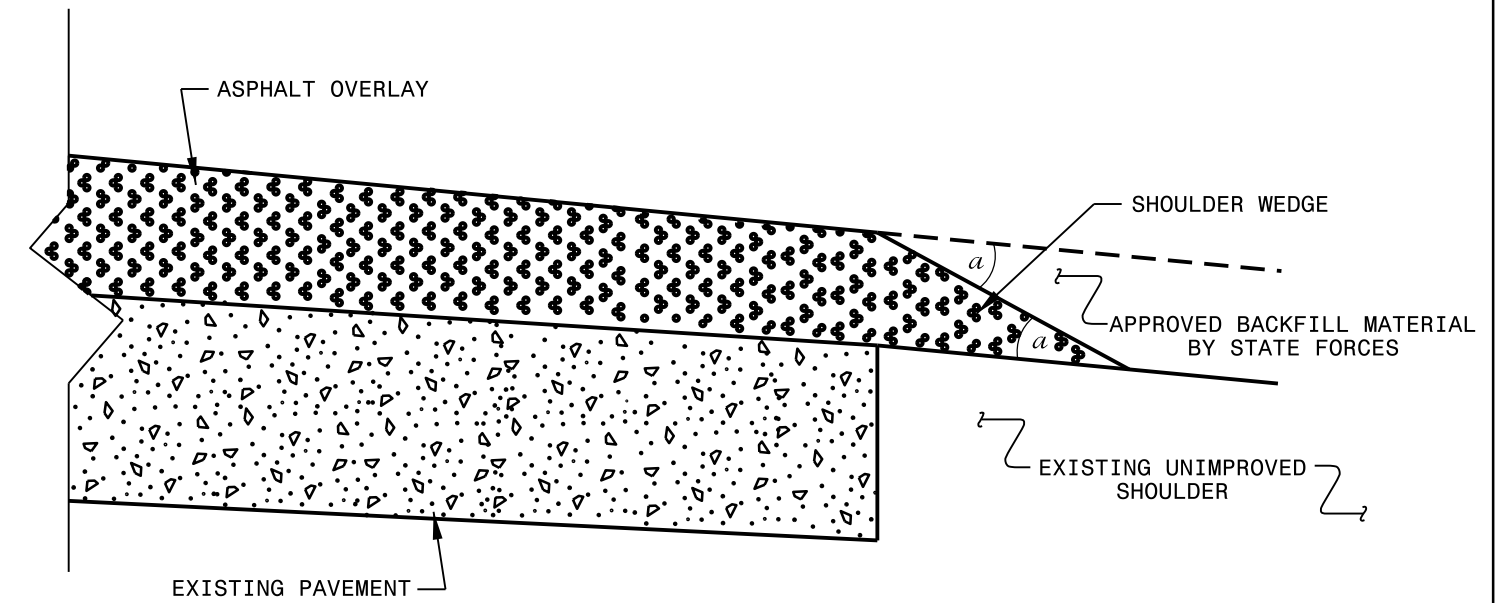
**NOTE:**

- SHOULDERS SHALL BE RECONSTRUCTED AS SHOWN IN STD. DWG. NO. 560.01 & 560.02, WITH A MINIMUM SLOPE OF 1" PER FOOT TO ENSURE POSITIVE DRAINAGE AWAY FROM THE ROADWAY.
- A VEGETATIVE BUFFER SHALL BE MAINTAINED BETWEEN THE DISTURBED AREA ALONG THE EDGE OF PAVEMENT AND THE DITCH SHOULDER POINT TO MINIMIZE EROSION. PULLING DITCHES OR CUTTING SHOULDERS TO GENERATE BORROW MATERIAL WILL NOT BE ALLOWED.
- REQUIRED BORROW MATERIAL MAY BE OBTAINED FROM NCDOT STOCKPILES. ANY EXCESS MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR IN AN APPROVED DISPOSAL SITE.

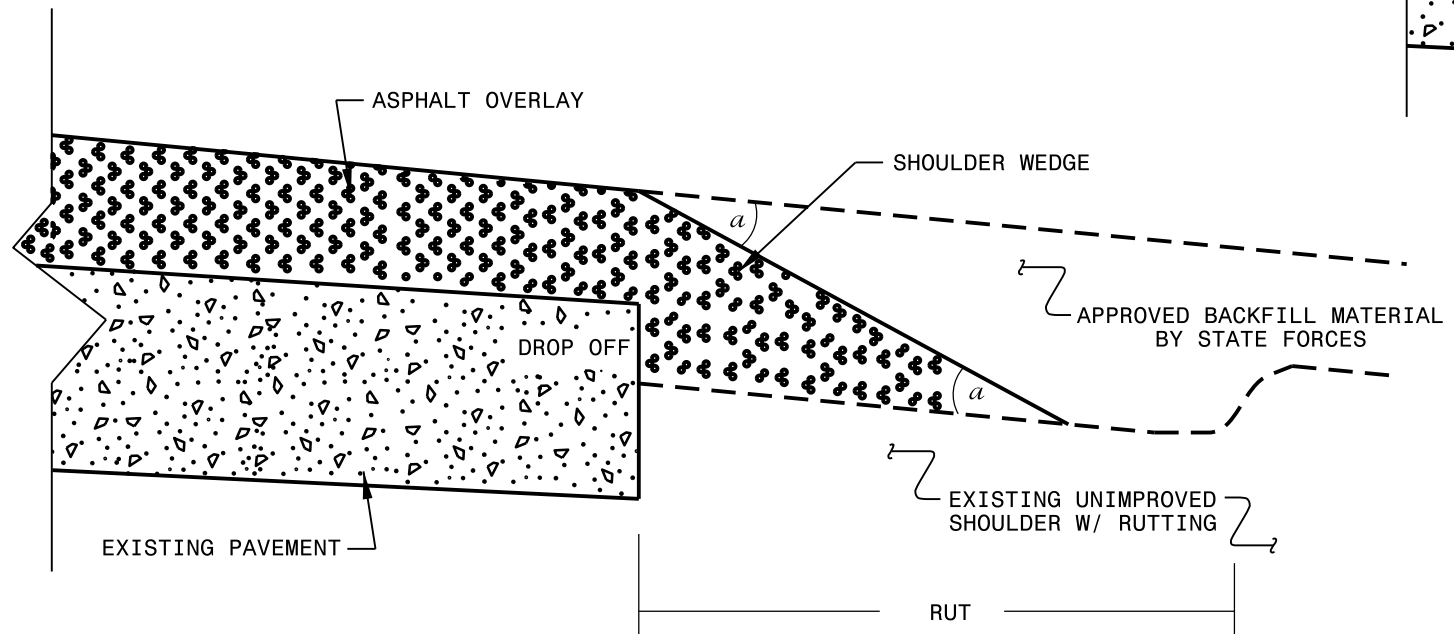
- NOTES:
- 1) DETAIL DOES NOT APPLY TO OGAFB AND ULTRA-THIN BONDED WEARING COURSE.
  - 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
  - 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS.



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Projects w/ Widening or  
 with Existing Paved Shoulder having no dropoffs)



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Projects w/ NO Widening)



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Adjacent to  
 Rutted Shoulder)

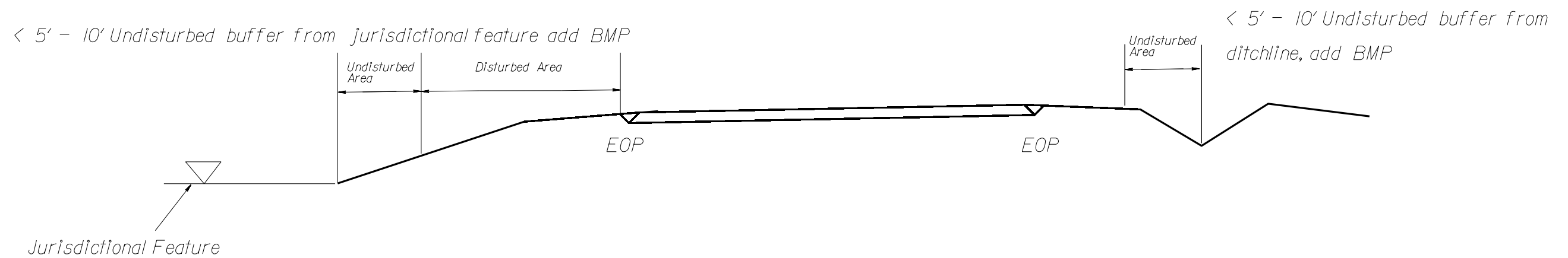
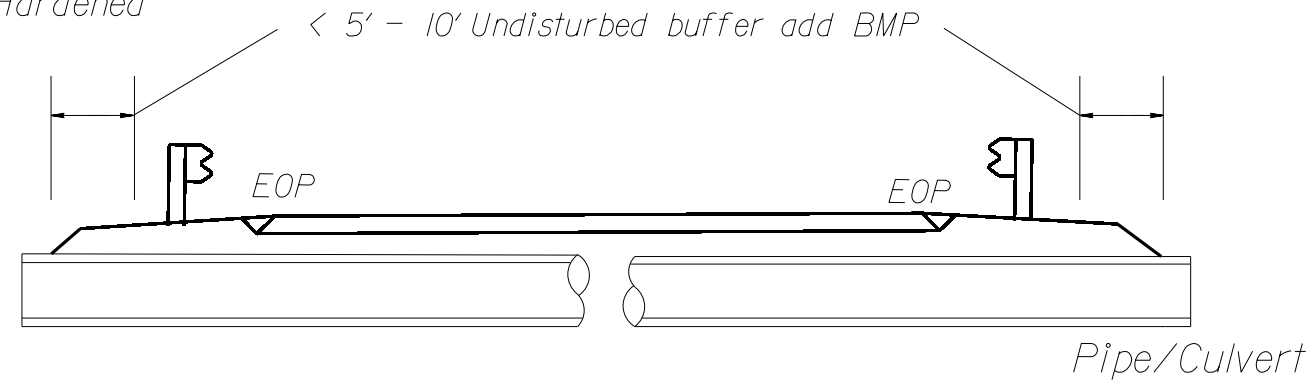
- SHOULDER WEDGE ANGLE = 30°

<b>CONTRACT STANDARDS AND DEVELOPMENT UNIT</b>	
Office 919-707-6950	FAX 919-250-4119
<b>SHOULDER WEDGE DETAILS</b>	
ORIGINAL BY: T.SPELL	DATE: 7-19-11
MODIFIED BY:	DATE: 10/16/12
CHECKED BY:	DATE:
FILE SPEC.: s:\usr\details\stand\shoulderwedgedetail.dgn	

NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

BMP Options: Wattle, Silt Fence or Hardened Aggregate.

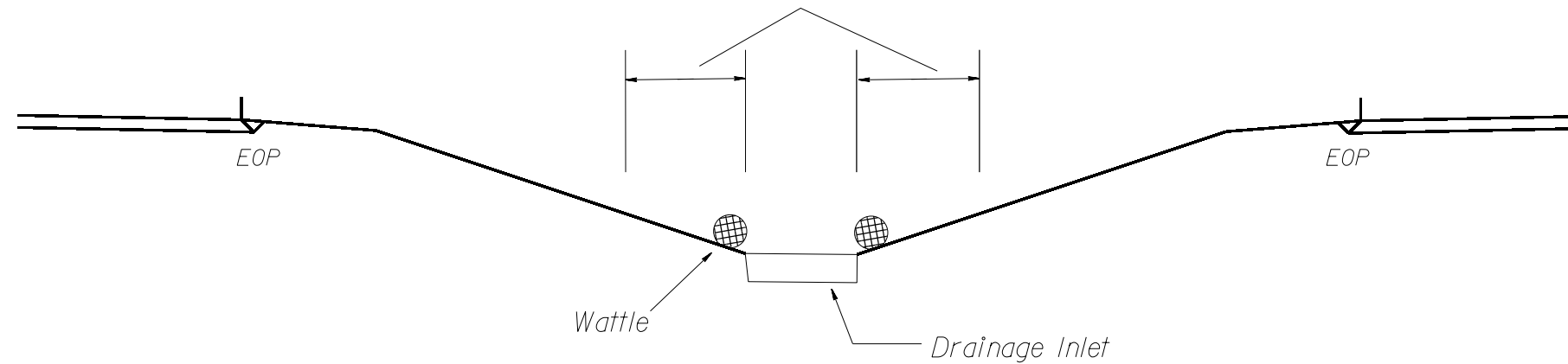
# EROSION CONTROL DETAIL



Use BMP's if shoulders and/or frontslopes and/or ditchline and/or backslopes are disturbed

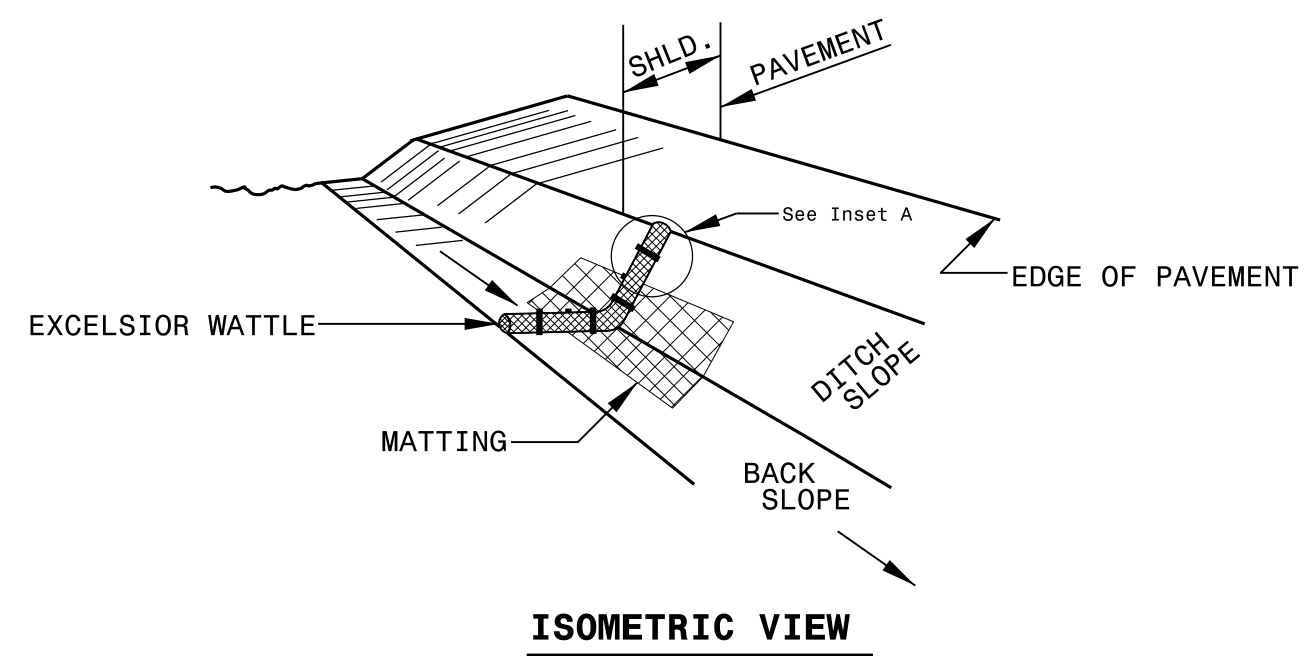


< 5' - 10' Undisturbed buffer from inlet, add wattle



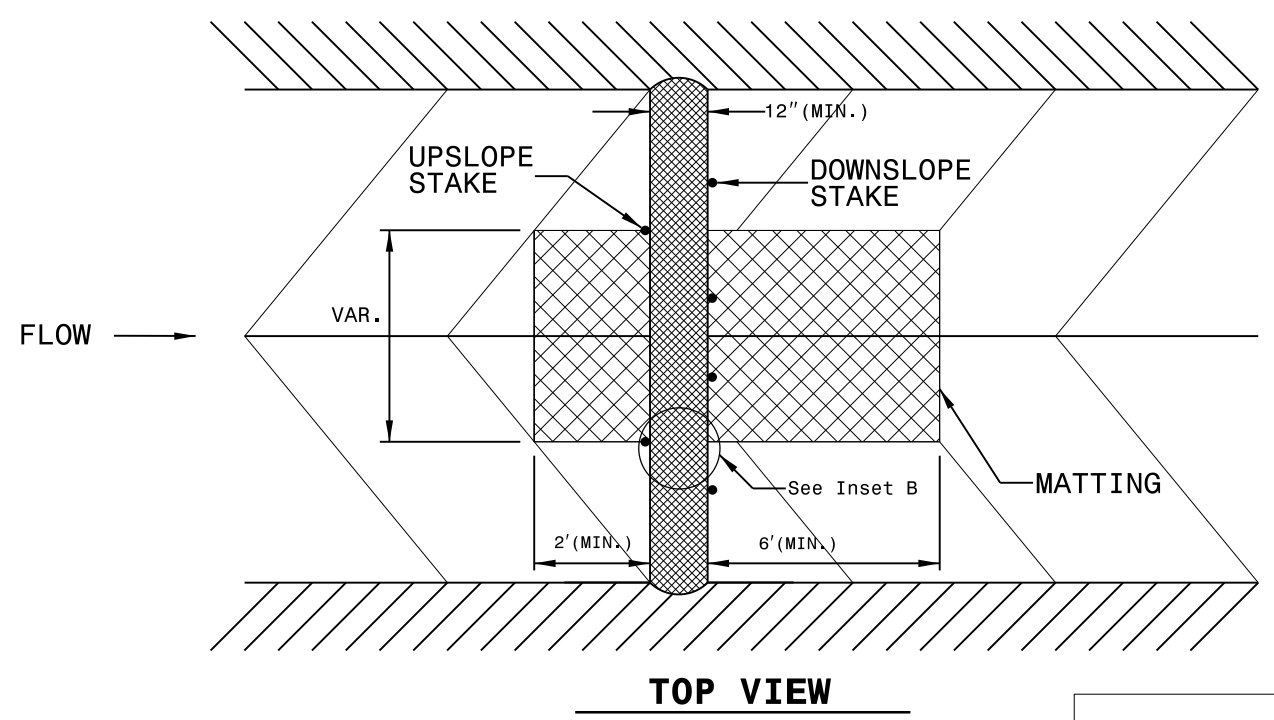
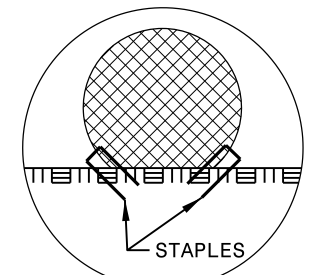
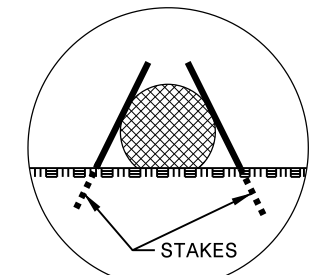
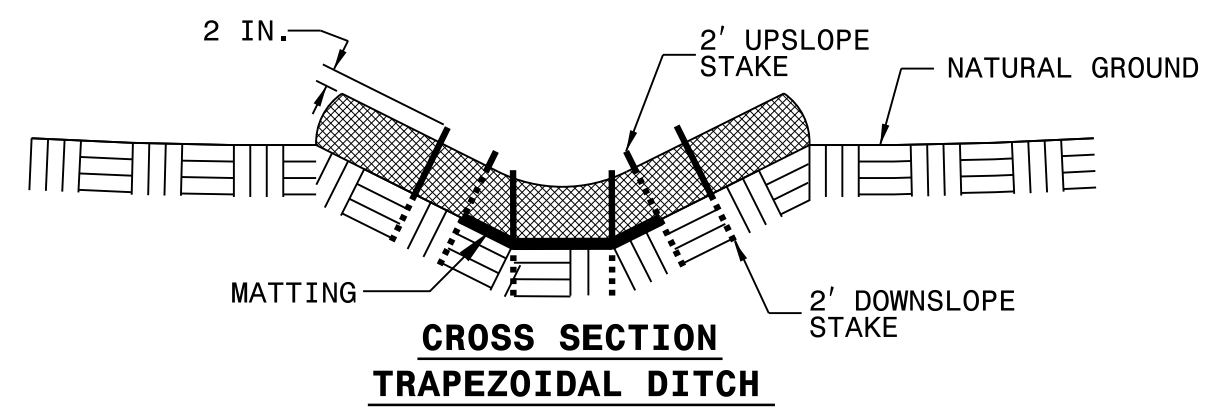
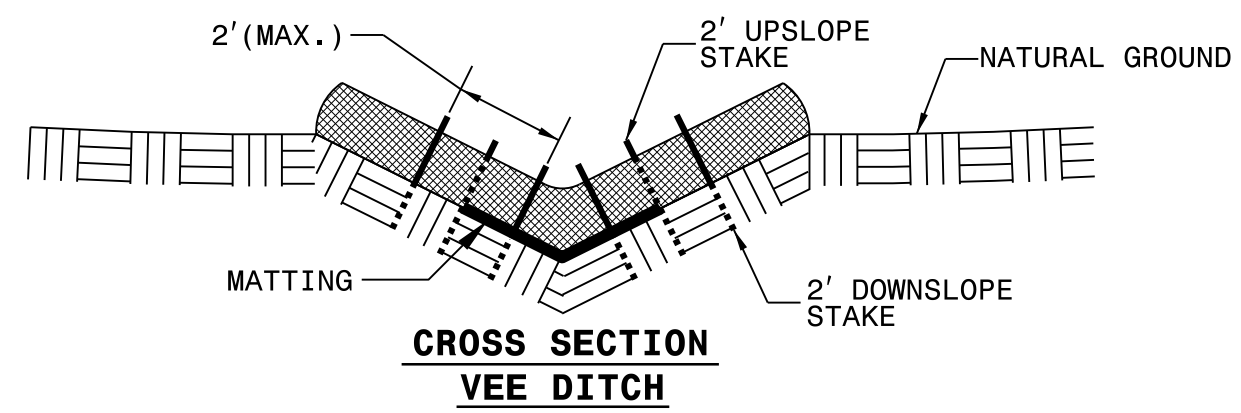
NOT TO SCALE

# WATTLE DETAIL



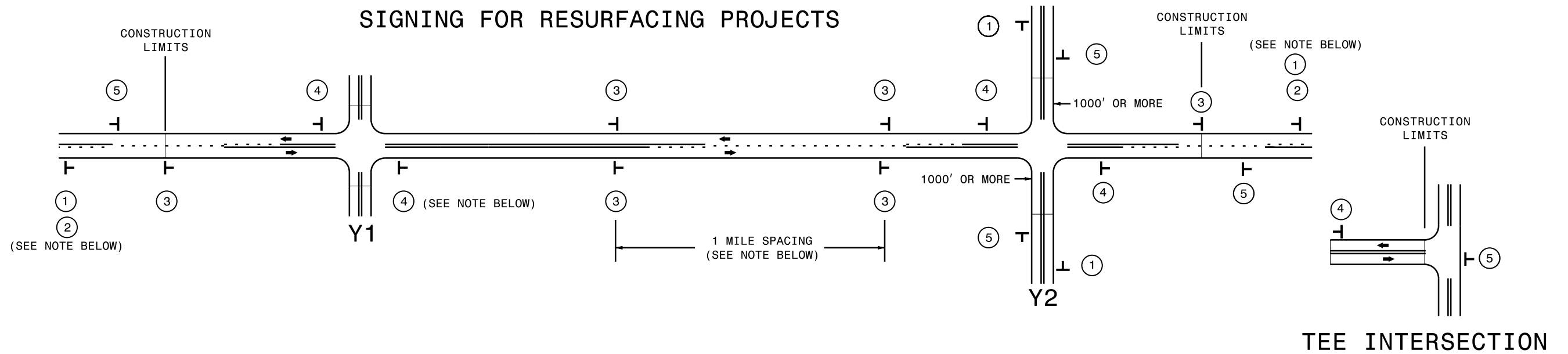
**NOTES:**

- USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.
- USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.
- ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.
- INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.
- PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.
- INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.
- INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.



NOT TO SCALE

# SIGNING FOR RESURFACING PROJECTS



LEGEND	
	STATIONARY SIGN
	DIRECTION OF TRAFFIC FLOW

## MAINLINE (-L-) SIGNING

## -Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	1	2	3	4	5	
						<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> <li>LESS THAN 1000' OF RESURFACING ALONG -Y- LINE</li> <li>SUBDIVISION ROADS</li> <li>DEAD END ROADS</li> </ol> <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div> <p>W20-1 48" X 48"</p> </div> <div> <p>W20-7 A 48" X 48"</p> </div> </div> <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p>
	<p>PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</p> <p>#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)</p>		<p>- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER.</p> <p>- AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.</p>			
			<p>- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS.</p> <p>- DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS.</p> <p>- INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE.</p> <p>- FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH.</p> <p>- A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</p> <p>- FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.</p>			
			<p>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.</p>			



RESURFACING  
ADVANCE WARNING SIGNS  
FOR  
RURAL AND SUBURBAN  
2 LANE ROADWAYS